

COUNTRY MEADOWS OUTLINE PLAN

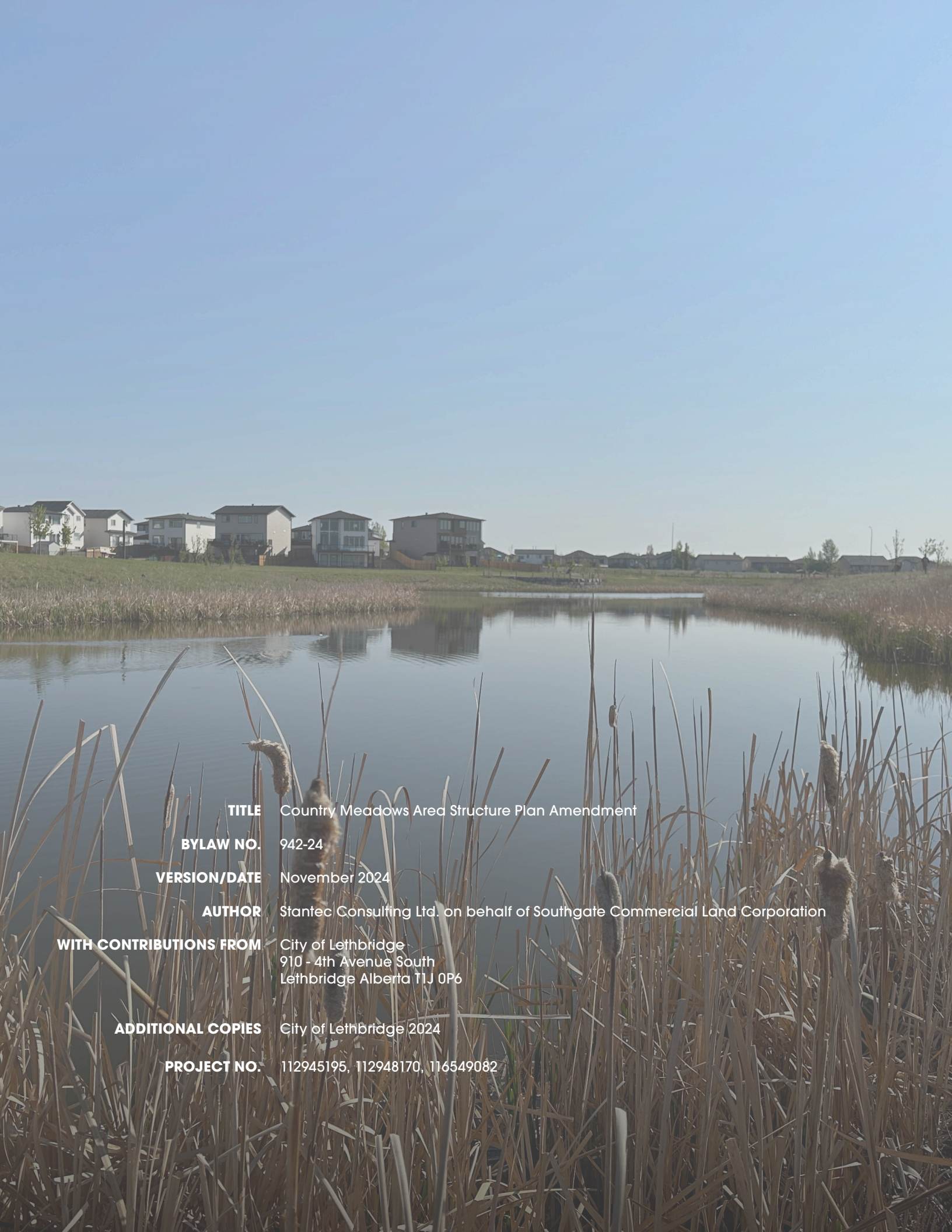
TECHNICAL ELEMENTS

PREPARED FOR:
SOUTHGATE COMMERCIAL LAND CORPORATION
LETHBRIDGE, ALBERTA

PREPARED BY:
STANTEC CONSULTING LTD.
LETHBRIDGE, ALBERTA



NOVEMBER 2024



TITLE Country Meadows Area Structure Plan Amendment

BYLAW NO. 942-24

VERSION/DATE November 2024

AUTHOR Stantec Consulting Ltd. on behalf of Southgate Commercial Land Corporation

WITH CONTRIBUTIONS FROM City of Lethbridge
910 - 4th Avenue South
Lethbridge Alberta T1J 0P6

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PROJECT NO. 112945195, 112948170, 116549082

Date: January 23, 2025

File No: OLP00018

PLANNING REVIEW TEAM - OUTLINE PLAN TECHNICAL ELEMENTS APPROVAL

RE: Country Meadows Outline Plan Amendment

During the Regular Meeting of City Council held on Tuesday, January 21, 2025 in City Council Chambers at 12:46 PM item 4.3.1 Recommendation from Assets & Infrastructure Standing Policy Committee – Country Meadows Outline Plan Amendment was approved by the following resolution:

BE IT RESOLVED THAT Council that Council:

1. Approve the amended Land Use Concept for the Country Meadows Outline Plan, titled Land Use Concept 2024 as provided in Attachment 2; and
2. Replace the Land Use Concept approved by the Municipal Planning Commission on March 16, 2019, titled Residential Land Use & Density, pages 17 through 21 of the Country Meadows Outline Plan.

Following the approval of the Land Use Concept by City Council, Planning Review Team voted on **January 23, 2025** to accept the accompanying Technical Elements.

Members present at the meeting were:

Adam St Amant (Transportation)

Aksharesh Parmar (Planning and Design)

Andrew Sommerville (Parks)

Angie Olsen (Planning and Design)

Byron Buzunis (Capital Planning and Engineering Services)

Gareth Jones (Lethbridge Electric Utility)

Janet Gutsell (Planning and Design)

Jason Price (Planning and Design)

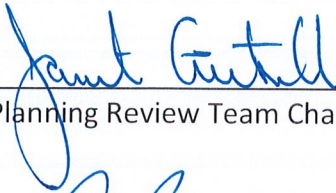
Mark Hilger (Lethbridge Electric Utility)

Mark Molesky (Capital Planning and Engineering Services)

Matthew Harker (Capital Planning and Engineering Services)

Sadia Munia (Transportation)

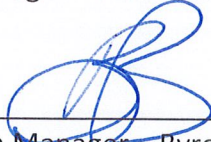
Tyson Boylan (Planning and Design)



Planning Review Team Chair- Janet Gutsell



Planning Manager – Jason Price



Urban Design Manager – Byron Buzunis. P. Eng.

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CHAPTER 1: INTRODUCTION

This Country Meadows Outline Plan (OLP) Amendment is being prepared on behalf of Southgate Commercial Land Corporation. **Figure 2.1: Area Context Plan** identifies this amendment area.

The Country Meadows OLP consists of approximately 123.10 ha (304.18 ac) of land within the Country Meadows Area Structure Plan (ASP), located in West Lethbridge.

~~While the original Country Meadows OLP encompassed an area of just over 300 acres in West Lethbridge, the proposed OLP Amendment area consists of portions of the NE ¼ SEC 33-8-22-4 and —an area of 87 acres. **Figure 2.2: Existing Land Use** highlights the original Country Meadows OLP area and the boundary of the OLP Amendment area. It should be noted that all related information in the OLP Amendment document refers to this indicated boundary.~~

The original Country Meadows OLP was approved by the Municipal Planning Commission in February 2012, and subsequently amended in March 2019. ~~they had also approved the first amendment to this plan.~~ This amendment will describe the proposed land use layouts and the corresponding impacts to the plan area. The design of the OLP ~~was~~ **is** consistent with the policies and intent of the Country Meadows ASP ~~and remains so with this OLP amendment.~~

The Country Meadows OLP Amendment has been completed to:

- Reflect current market trends and provide flexibility in future zoning through the removal of prescriptive land use designations.
- Reconfigure the land on the west side of the amendment area to incorporate an additional 15m of road right of way back into developable lands.
- Update unit and population statistics based on the future removal of an ATCO pipeline (expected 2025).
- Describe adjustments to the local road network, storm water management, and proposed phasing necessitated by these revisions.
- Refine the land use plan for greater efficiency and ensure future affordability.

CHAPTER 2: LOCATION & AREA CONTEXT

2.1 Location

The Country Meadows Outline Plan area falls within the Country Meadows Area Structure Plan.

The subject lands are bounded on the east by ~~the future~~ Metis Trail West, on the south by ~~the future~~ Garry Drive West, on the north by Walsh Drive West and on the west by the future Chinook Trail. The Country Meadows Outline Plan is situated west of the existing West Highland's community and north of ~~the future community of~~ Garry Station. Directly to the north of Country Meadows, ~~plans are being developed for a~~ is an approved commercial area "The West Lethbridge Employment Centre."

2.2 Topography

The subject lands consist of gently undulating prairie landscape that is typical of the Lethbridge region. In general, the lands slope from a centrally located plateau in all directions, with a maximum elevation difference of approximately 12-13m. For further information, **Appendix C: Geotechnical Evaluation** is included. Additionally, **Appendix D: Phase 1 Environmental Site Assessment** was also completed for these lands.

~~Appendix C — contains the Geotechnical Evaluation.~~

~~Appendix D — contains the Phase 1 Environmental Site Assessment.~~

2.3 Existing Ground Disturbance- Geotechnical

Particular attention should be given to areas of existing development (farmsteads, dugouts, existing underground utilities, septic fields, solid waste pits and/or burn pits, etc.) Existing dugouts should be drained, all saturated material removed and backfilled with general engineered fill. At subdivision and detail design these features (dugouts and wetlands) will be identified and remediated under the supervision of a geotechnical engineer. All existing utilities (whether operational or abandoned) must be located. Existing utility trenches pose a particular risk due to settlement of backfill material. Care should be taken to ensure that all existing utility trenches are excavated to remove the utility and backfilled with general engineered fill. All other existing or historical ground disturbances should be removed and backfilled with general engineered fill. For further information refer to **Appendix C: Geotechnical Evaluation**.

2.4 ATCO Pipeline Relocation

The potential relocation of the ATCO pipeline has been discussed since the original adoption of the Country Meadows Outline Plan in 2012. Through detailed conversation with ATCO the lines bisecting the Plan Area have been divided into three segments as noted within **Figure 9.5 ATCO Transmission & Distribution Plan**.

Any further consideration for removal or relocations of these lines should be pursued with ATCO in order to remove unutilized infrastructure from the developable lands. For further information, see **Section 9.4: Shallow Utilities**.

2.5 Existing Land Use & Zoning

The ~~existing~~ land use of The Country Meadows Outline Plan was ~~previously~~ classified as ~~predominately~~ Agricultural which provided for cropland and other suitable agrarian endeavors.

The ~~undeveloped portion of the~~ subject lands are currently designated Future Urban Development District (FUD) ~~on the eastern parcels of the plan area~~ and Direct Control District (DC) Bylaw 4590 ~~on the western parcel~~. The intent of the ~~FUD Future Urban Development District~~ is to protect lands for future development and subdivision once appropriate servicing and planning policies have been implemented. The ~~DC Direct Control~~ District, approved by Lethbridge City Council in 1993 permitted the subdivision of the quarter section into two equal parcels, allowing one dwelling unit for each parcel.

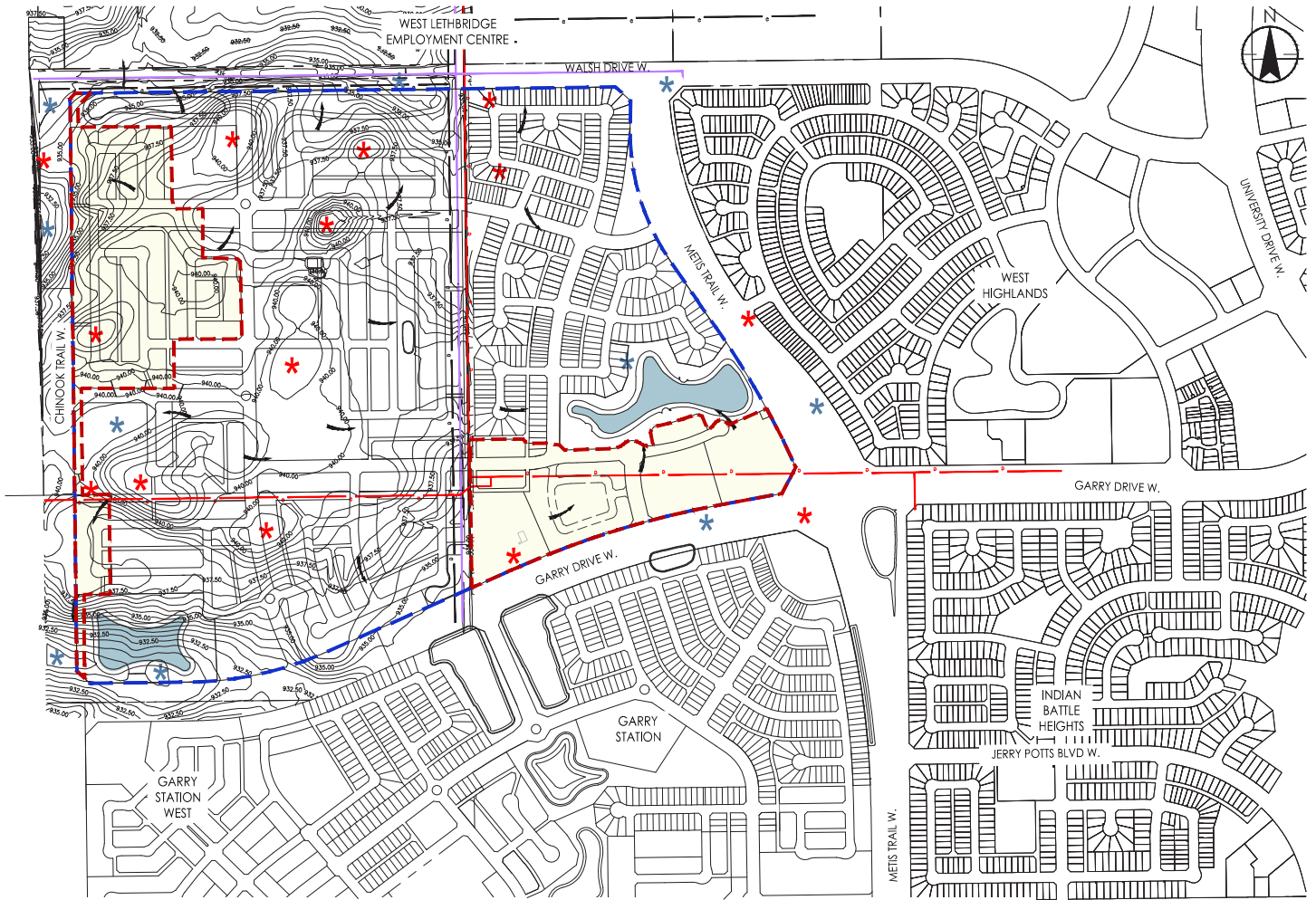
The surrounding land uses consist of existing residential housing to the east in the community of West Highlands; planned and approved residential development to the south in the development of Garry Station.

Figure 2.1: Area Context Plan – illustrates the location of Country Meadows within West Lethbridge

Figure 2.2: Existing Land Use – illustrates the current Land Uses within and surrounding the plan area.

~~The proposed Country Meadows OLP Amendment area consists of the NE ¼ SEC 33-8-22-4 – an area of 142 acres in West Lethbridge within the Country Meadows OLP area. **Figure 2.1: Area Context Plan** and **Figure 2.2: Existing Land Use** have been updated to include this OLP amendment.~~

Figure 2.1: Area Context Plan



Legend





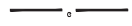





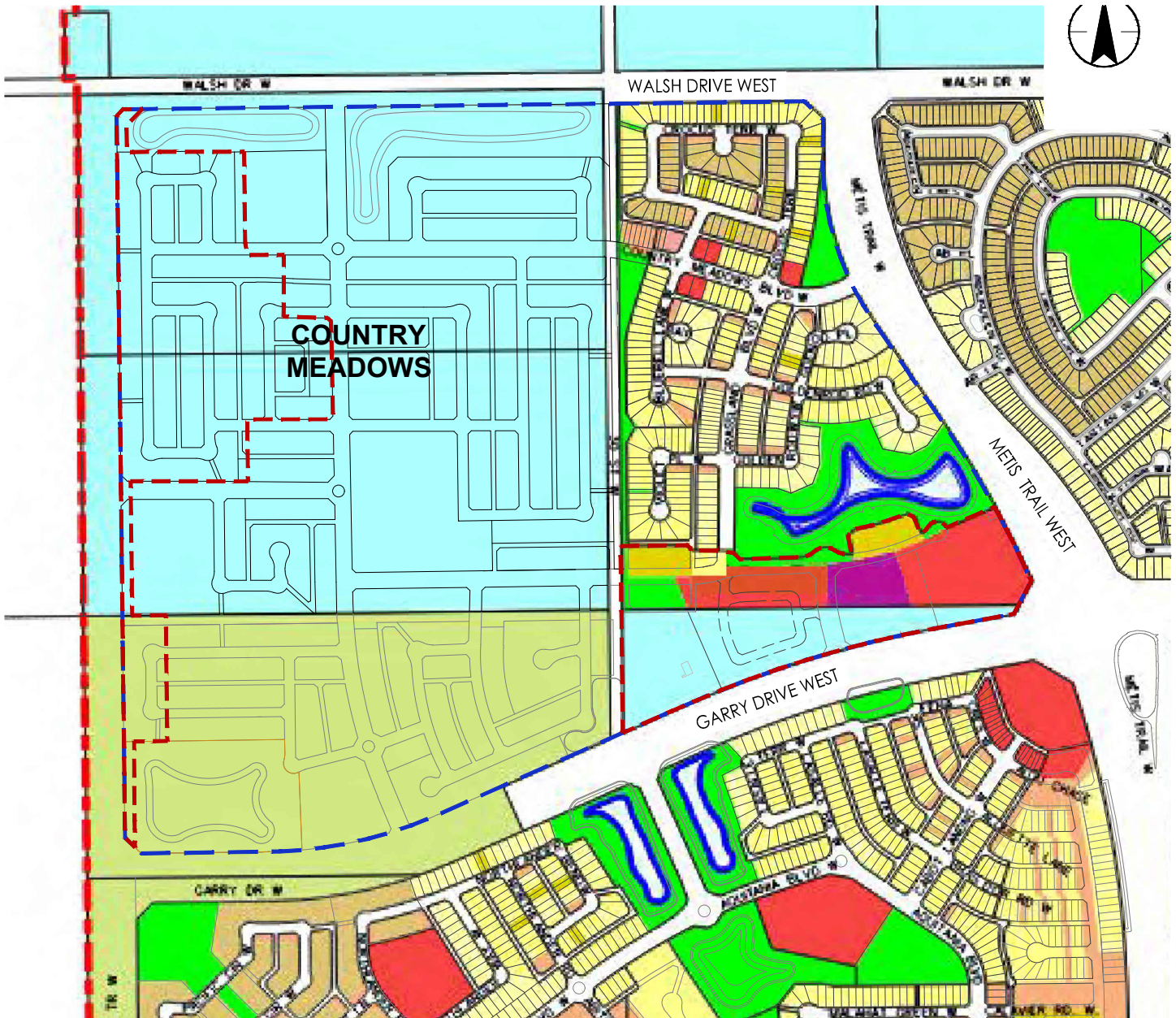
- | | | | |
|-------------------------------------------------------------------------------------|------------------------------------|-------------------------------------------------------------------------------------|---------------------------------|
|  | Country Meadows Boundary |  | Overland Flow & Spill Direction |
|  | Country Meadows Amendment Boundary |  | Existing Ground Contours |
|  | ATCO Gas |  | Low Point |
|  | ATCO Pipelines (High Pressure) |  | High Point |
|  | Telus | | |
|  | Fortis - Over head line | | |

Figure 2.2: Existing Land Use



- Legend
- Country Meadows Amendment Boundary
 - Country Meadows Outline Plan Boundary

CHAPTER 3: LAND OWNERSHIP

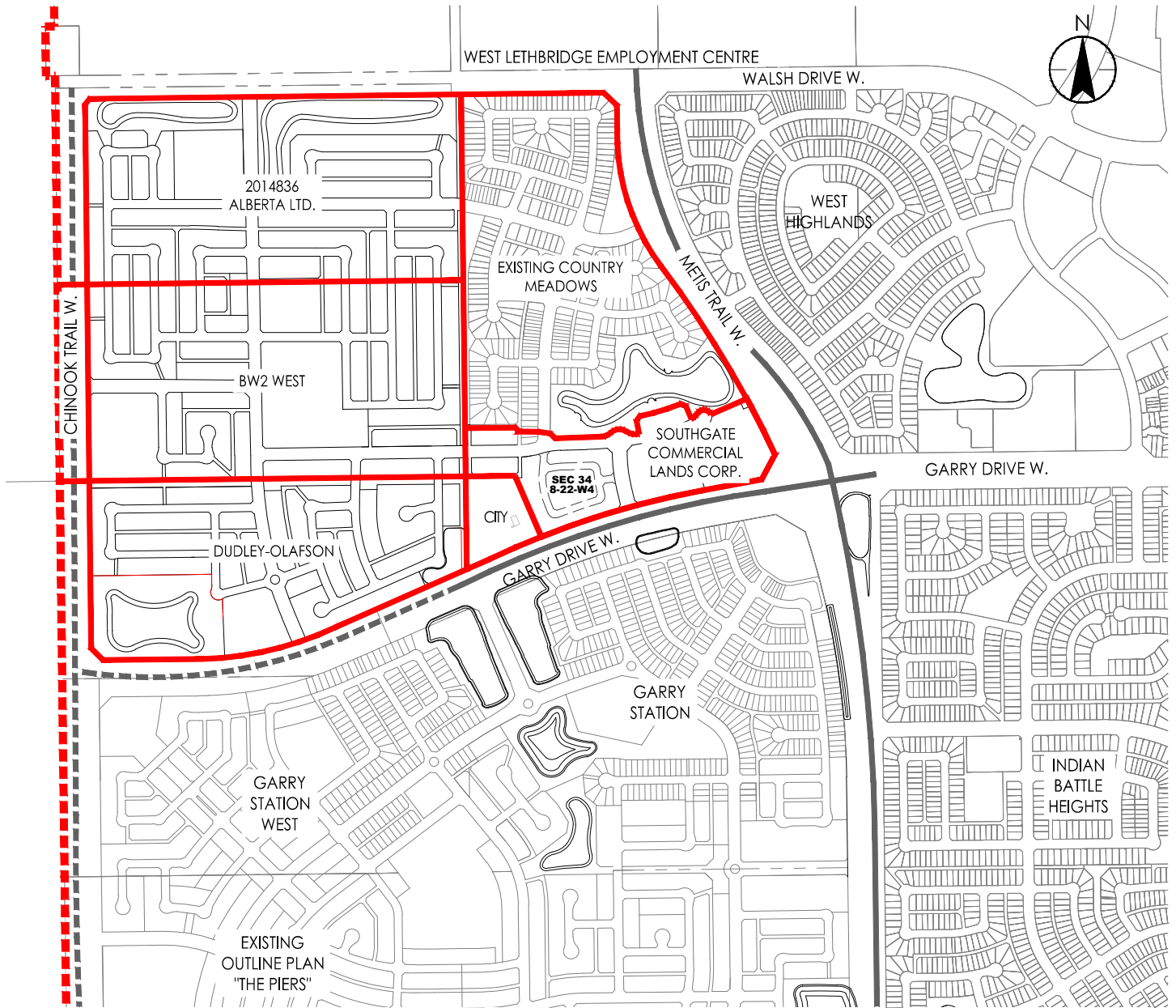
The Country Meadows Outline Plan area consists of 6 separate ownership parcels. The following outlines the **historic** land ownership and legal descriptions for the parcels within the Country Meadows Outline Plan boundary. **Figure 3.1: Land Ownership** identifies the ownership within the plan boundary and has been adjusted to incorporate additional lands on the west edge:

- Existing Country Meadows formerly owned by Southgate Commercial Lands Corp. – Portions of the NW Quarter of Section 34, Township 8, Range 22, West of the Fourth Meridian containing 27.96ha± (69.14 acres±).
- Southgate Commercial Lands Corp. – Portions of the NW Quarter of Section 34, Township 8, Range 22, West of the Fourth Meridian containing 9.50 ha± (23.47 acres±).
- 2014836 Alberta Ltd. – the North Half of the NE Quarter of Section 33, Township 8, Range 22, West of the Fourth Meridian containing 28.84 ha± (71.26 acres±)
- BW2 West – the South Half of the NE Quarter of Section 33, Township 8, Range 22, West of the Fourth Meridian containing 30.57 ha± (75.74 acres±).
- Debra L. Dudley-Olafson – the North Half of the SE Quarter of Section 33, Township 8, Range 22, West of the Fourth Meridian containing 24.16 ha± (59.68 acres±).
- City of Lethbridge – Lot 1, Block 1, Plan 0814008 containing 2.05ha± (5.56 acres±).

The Certificates of Title have been provided in **Appendix A: Certificates of Title**.

Figure 3.1: Land Ownership identifies the ownership within the plan boundary:

Figure 3.1: Land Ownership



- Legend
- ■ ■ ■ City of Lethbridge Limits
 - Country Meadows
 - - - - Future Arterial Roads
 - Existing Arterial Roads

CHAPTER 4: POLICY CONTEXT

The Country Meadows OLP represents the next detailed level of planning within the framework of an overarching Area Structure Plan (ASP). The ASP ~~That Plan—the Country Meadows Area Structure Plan—~~ was approved by City Council, Bylaw 5629 in February 2010. The ASP document provided a general land use concept for the 123.10 ha that comprise the Plan Area, a servicing strategy, a proposed transportation network and a phasing scheme for the development of the lands. The guiding policies for the development of the Country Meadows OLP are based on the principles set out in the Country Meadows ASP. The ~~OLP Outline Plan~~ is also based on a number of related and complementary policies that comprise part of the City of Lethbridge's ~~Integrated Community Sustainability Plan~~/Municipal Development Plan (Bylaw 6265, 2021).

These policies and their relationship to the development of the Country Meadows ~~OLP Outline Plan~~ are as follows:

1. MDP Policy 58

PROMOTE affordable housing by encouraging and facilitating the adequate supply of housing for all income groups.

1.1. ASP Principle

Establish a range of residential housing choices for various family types and for individuals of a range of ages and incomes, including single-family dwellings, medium density dwellings and senior-aged oriented dwellings and assisted-living facilities.

- The densities proposed in this plan range from single family (with lanes and without), through to townhouse and apartment units, providing significant choice for home ownership as well as renters - from single detached units to apartments.
- The developer wishes to ensure that buyers of homes in Country Meadows have the widest possible choice of housing types and building styles. Lot purchasers will therefore not be limited to having houses constructed solely by any “builders’ group” carrying out home building in Country Meadows. Purchasers will be permitted to select their own builder and develop custom homes, provided these meet the Plan’s architectural design standards.

2. MDP Policy 83 & 100

ENSURE opportunities for healthy living by incorporating a range of active and passive recreational opportunities into the open space system

PROMOTE walkable neighbourhoods by encouraging:

- A greater mix of appropriate land uses and infill development through policies in Area Redevelopment Plans.
- Growth areas to provide for a greater mix of land uses in Area Structure Plans and Outline Plans.
- Development of accessible housing units in areas where ancillary neighbourhood facilities are currently available (e.g. schools, parks, transit routes, groceries) or will be developed in the future, through land use plan preparation or consideration of applications for a change of land use.
- Area Redevelopment Plans for areas with inadequate green space to identify how the supply can be increased and how this can be paid for.

- Street-fronting and neighbourhood-oriented commercial development in new or existing neighbourhoods with a grid or modified grid street layout, through supporting appropriate land use amendments and identifying appropriate locations in Area Redevelopment Plans and Outline Plans.
- Commercial development around higher density residential areas, in Area Redevelopment Plan and Area Structure Plan preparation, and Land Use Bylaw amendments.

2.1. ASP Principle

Develop an integrated open space network, which creates a walkable and accessible environment, passive and active recreational amenities, and a highly aesthetic community thematic design which focuses on natural green and water oriented amenities.

- The open space system of the Country Meadows OLP is comprised of several diverse but interrelated components including the potential for an elementary school site with related amenities, a neighborhood park, a linear park system, a number of roundabouts providing a traffic calming effect to the plan area and promoting walkability.
- Open space (both creditable and non-creditable) accounts for almost 19% of the gross developable area in Country Meadows with land dedicated to a mix of uses including a school site, recreational opportunities, stormwater management, and public utilities.
- Streetscapes and storm water management facilities provide space for both active and passive recreation. Combined, these components will offer a significant range of recreational opportunity to the community. At the same time, accessibility to these sites will be enhanced through pathway and sidewalk linkages. The extensive pathway system – both local and regional - will offer an environment that not only facilitates walking and cycling, but encourages it.

3. MDP Policy 107

PROMOTE orderly growth by:

- Discouraging non-contiguous growth and expansion of the built environment within the City.
- Encouraging an adequate supply of land that is planned and available for servicing to meet market demand.
- Encouraging growth patterns that maximise the use of existing infrastructure and services in order to avoid or delay the construction of new infrastructure.
- Including a range of choice of new expansion areas for residential, commercial and industrial development.

3.1. ASP Principle

Establish a land use strategy that is practical, effective and cost efficient to facilitate development through strategic land use location and logical extension of servicing infrastructure.

- Servicing of the Country Meadows plan area is based on a logical and orderly extension of services from adjacent lands. The plan area is contiguous with other development areas that are either developed already or are scheduled for development. Servicing connections for storm, and sanitary requirements will be made via the adjacent West Highlands community. Long term needs for sanitary service will be provided in accord with the City's capital development schedule.
- The arrangement of land uses has taken into consideration the long-term needs of customers and the market. The central location of the elementary school provides minimal walking distances for students in the entire plan area. The commercial site is located to ensure maximum visibility and access from Garry Drive. The largest number of medium density sites has been located adjacent to open spaces to provide recreation opportunities for the highest concentration of residents. All of these uses are in the path of logical infrastructure servicing.

4. MDP Policy 113

PROMOTE a sustainable development pattern which makes efficient use of land, minimises the need for motorised travel and facilitates social cohesion, by encouraging:

- The design of live, work, shop and play land uses in proximity to one another.
- Mixed-use development and a mix of land uses in existing and future commercial areas.
- The design of the built environment to facilitate walkability and rollability by providing complete networks of accessible sidewalks and crossings throughout the city.
- The design of neighbourhoods to minimise driving distances and reduce automobile trip generation, through the use of grid or modified grid street layouts (where topography and storm water management solutions allow) and a mix of land uses which aims to allow residents to meet their daily needs within a 750 m walk of their homes.
- A diverse range of housing forms and price points to be incorporated in all new neighbourhoods.
- Mixed-use development in residential neighbourhoods, in locations which maximise commercial viability and ease of access for nearby residents.

- The creation of city and neighbourhood focal points that provide opportunities for community gathering, and that encourage interaction between all age groups and abilities.
- Neighbourhood design and public spaces to mitigate the impact of climatic extremes (temperature, wind, drifting snow).
- Throughout the city, architecture and streetscaping which contribute to a sense of place and civic pride.

4.1. ASP Principle

Create variety in both residential form and lot type to enhance choice and foster diversity and visual interest.

Create a mixed-use community which is primarily residential in nature but includes the essential community services and amenities needed to create a complete neighbourhood.

- The Country Meadows Outline Plan is characterized by a mix of uses and amenities intended to meet the needs of a new and thriving neighbourhood. These include a range and variety of residential densities and housing types, a centrally-located school, and a potential mixed use site. The parks and open spaces of this plan – including an extensively linked linear park system - further contribute to this variety of land uses and to creating a quality living environment for future residents.
- Development statistics within the Country Meadows Outline Plan boundary are as follows:
- A diverse range of housing opportunities are provided by a large range of residential zoning categories. Among these categories is the R-CL zone which was specifically included to encourage both innovative and visually creative projects in the community. The small parcel district (R-SL) has been included to permit wider lot choice. To ensure that this new community will

meet the needs of many age groups, the medium density districts selected for inclusion in the Plan (R-37 and R-75) allow for senior citizen housing. Although no sites have been specifically selected for seniors housing, the medium density sites provided in the Plan will offer opportunities in a variety of locations.

- The Country Meadows Outline Plan will strive to provide a visually appealing community. To achieve this, the Plan includes architectural standards that will apply to the housing projects throughout the neighbourhoods. Split rail fencing will transition to screen fences along roadways, fieldstone pillared arbour entry features will complement other community features, and natural timber elements, native grasses and flora will enhance the farming/ranching lifestyle. The open space system will meander throughout the community and bring focus to a gathering gazebo that will add character and aesthetic appeal to the plan area.

5. MDP Policy 118 & 134

PROMOTE easy access to public transportation by strengthening the coordination of land use with transit planning.

ENSURE the improvement of pedestrian and cyclist access to destinations both within and outside of neighbourhoods, by incorporating appropriate infrastructure in planning for new and existing neighbourhoods.

5.1. ASP Principle

Provide a neighbourhood commercial area and a school site serving local residents via both pedestrian and vehicular connections.

- The Outline Plan includes both a public elementary school and a neighbourhood commercial site. Both sites are accessible by private as well as public transport. The proposed public transit routing system and the transit stops offer convenient service to the commercial parcel and the school as well as the community as a whole. A 1.5 km linear park system that takes advantage of the ATCO gas line easement acts together with other connections to form both a local pedestrian/cycling system in the community and a means of connecting to the regional pathway.
- Approximately 7.0 acres (2.8 ha) of “linear” open space will be provided for pedestrians and cyclists.
- Sidewalks complement the linear park system and ensure pedestrian access to all the major destinations both within and outside the plan area.

CHAPTER 5: COMMUNITY VISION & DESIGN

In the early stages of the Country Meadows Outline Plan process, landowners and their consultants met with City Administration. The purpose of this meeting was to begin the process of establishing a direction for the community's vision and theme. Please refer to **Appendix J: Gate 2 Sign-Off** for this preliminary Visioning document. The workshop also focused on the amenities, elements, ideas, must-haves, and concepts that enhance a community. Key themes emerged that were incorporated into the vision and design of the community included:

- Quality, yet affordable and practical variety of homes
- Innovative design that establishes community identity and character
- Owner and community ownership of sustainable practices
- Inclusion of active and passive parks, green spaces and community linkages
- A safe and mobile community

5.1 Community Vision

Country Meadows, a pioneer's destination after a long journey west, is a logical extension of a theme developed in West Lethbridge since 2000---the early settlement of Western Canada.

From the Atlantic Crossings when new immigrants arrived at the Piers on Canada's Eastern shore, settlers from the east travelled by rail or wagon to western destinations like a Garry Station. Surrounding these destinations, communities would develop, and a new "country" style of living would be born.

The Vision for Country Meadows is the bringing together of a diverse multitude that developed their own unique style based on the natural materials at hand in Western Canada. The use of timber and fieldstone would become a hallmark of the farming and ranching lifestyle. However, there were those settlers whose lives were defined by a more manicured eastern style. Two story homes painted white with picket and split rail fencing. Visitors to these homes might pass beneath an arbour; in the evening, neighbours might gather at a community gazebo to share thoughts of their day.

In many respects, these two distinct styles came together and created a new landscape in and around Lethbridge and this is a key part of the vision of Country Meadows.

On the eastern half of the Country Meadows, a pond and wetlands will be the focal gathering point for the community. Split rail or cross-buck fencing in open spaces will transition to residential screen fences along arterial roadways; arbours will be incorporated at open space entrances. Fieldstone pillars will provide an anchor to fence lines and other features. Features such as a gazebo and canoe launch will allow residents to enjoy a range of leisure activities.

As development progresses west, it is anticipated that this estate country style might transition subtly with the introduction of timber elements indicative of a ranching/farming style. Fieldstone will be the anchor between the east and west side of Country Meadows as well as native prairie grasses and flora.

Inspired by a prairie landscape with expansive views, Southgate Commercial Lands Corp. representing six landowners, have teamed together to create a community representative of the pioneering spirit driven by family values.

Country Meadows will be a community built into the rolling prairies, where exploration along a network of pathways and inter-connecting green spaces hails a sense of wonder as the scenic Oldman River Valley welcomes them to a new place called home.

5.2 Community Design

A Visioning Workshop resulted in unique elements and characteristics that were observed throughout the discussion and design charette including:

- A school site as a central hub for the community
- A strong connectivity among people that exhibited pronounced pedestrian and bicycle linkages
- Varied amenities that are well spread throughout the community
- Creative approaches to establish innovative opportunities from presently viewed constraints

The Country Meadows Outline Plan which evolved from the visioning workshop to the conceptual stage incorporated the above elements into a comprehensive community plan.

The concept is based upon a cellular grid system with a central elementary school at its core. The principal land uses in Country Meadows are residential. The arrangement of land uses recognizes the continued strong local demand for suburban style single detached homes but will also include more affordable multi-family sites. The mixed use area will include neighbourhood stores, zones of multi-family and low density residential. The plan will also incorporate ~~Urban Innovation Zones that will be~~ integrated residential ~~communities~~ around small parks.

CHAPTER 6: OPEN SPACE LAND USE

6.1 Open Space

The Open Space system within Country Meadows has been comprehensively designed to incorporate the ideas, concepts, and elements identified in the design workshop and the principles and objectives of the Area Structure Plan. Materials and elements used in the plan area will harmonize with the vision and theme as discussed in **Section 5: Community Vision & Design**. The entry feature **could include** fieldstone pillar arbours along with native prairie grasses and flora provide a theming anchor throughout the community and split rail or cross-buck fencing transitioning to screen fences will be incorporated in open spaces. Timber elements **will may** enrich the estate country style amenity features. The following summarizes the main amenities of the open space system as shown in **Figure 6.1: Open Space Network**:

Neighbourhood Park

The Neighbourhood Park will be designed for more passive recreational activities and serve as community gathering points. Some of the key elements planned for these parks include:

- Gazebos or Open Air amphitheaters
- Pathways
- Natural Prairie Grasses and Wetlands
- Playground Features
- Natural Play Areas

Neighbourhood parks with wet ponds should be planned to take advantage of pond water irrigation through a central pump station as well as use make-up (canal water) to top up the pond during drought conditions. Water line connections between parks areas should be installed through walkways and green belts but should limit conflict areas such as within carriage ways.

Potential School Site

The potential for a school site that is centrally located in Country Meadows has been provided. Currently the site is unassigned to any specific school district. The site could include a modern school building and a variety of recreational amenities which may include playground equipment, basketball court, and youth soccer pitch. The final programming requirements will be determined in consultation with school officials to ensure their needs are met. Should the parcel not be developed into a school site, a centrally located open space with unique amenities could be completed. Downsizing of the site, from the original plan, has been completed to align with current school site sizing trends within the City of Lethbridge.

Frontage Parks (Modified Pocket Park)

~~The design of these parks is integrated with the adjacent Urban Innovation (UI) District.~~ The centrally located unique park design has housing fronting onto the park space, replacing the street with a pedestrian and bicycle corridor, creating a more pedestrian friendly environment as an alternative to conventional design where housing typically backs onto park space. Additionally, a second pocket park will function as a tot lot park featuring naturalized playscape features. The parks provide a visible amenity to residents walking to the northwest wet pond and creates a small MR pocket that will be linked to pathways and sidewalks. Comprehensive planning of this park will be required during subdivision ~~of the UI parcel.~~

Linear Parks

Linear Parks have been created along existing infrastructure channels and it is anticipated that they will provide interconnectivity between the community, elementary school and larger open spaces through local connector pathways. The connections will be designed to accommodate pedestrians/cyclists and promote alternative transportation within the community. **All grading and construction of the linear pathway shall be completed by the developer.**

Roundabouts

Landscaping treatment of roundabouts will be finalized during detailed design at the subdivision stage.

Storm Water Management Facilities

The storm water management facilities have been incorporated into the open space designs throughout the community. The facilities will be utilized as an aesthetic amenity and function to enhance a sense of arrival and provide a visual park experience.

6.2 Connectivity

The visioning workshop identified a strong desire to create an efficient network of pathways to promote walkability and encourage alternate modes of transportation. The park network connects people to each area and is supported by strong pedestrian and bicycle routes throughout the community by local connector pathways.

The Country Meadows pedestrian network connects to the existing pathway to the Garry Station development and regional multi-use pathway to provide a strong pedestrian linkage south to the core commercial, educational and recreational amenities. This multi-modal linkage will provide residents in both communities with alternative modes of transportation to and from shared amenity spaces.

6.3 Reserve Dedication Analysis

The provision of public parks and open space within Country Meadows represents ~~10.1%~~ 10.13% of the net developable land. Final Municipal Reserve dedications will be finalized through subdivision process and payments of cash in lieu of dedicated space may be utilized by the developer, if required. A summary of the total reserve dedication for Country Meadows is as follows:

Figure 6.1: Open Space Network – illustrates the prominent neighbourhood and open space that connects to the city’s bikeway and pathway network.

Table 1: Reserve Breakdown

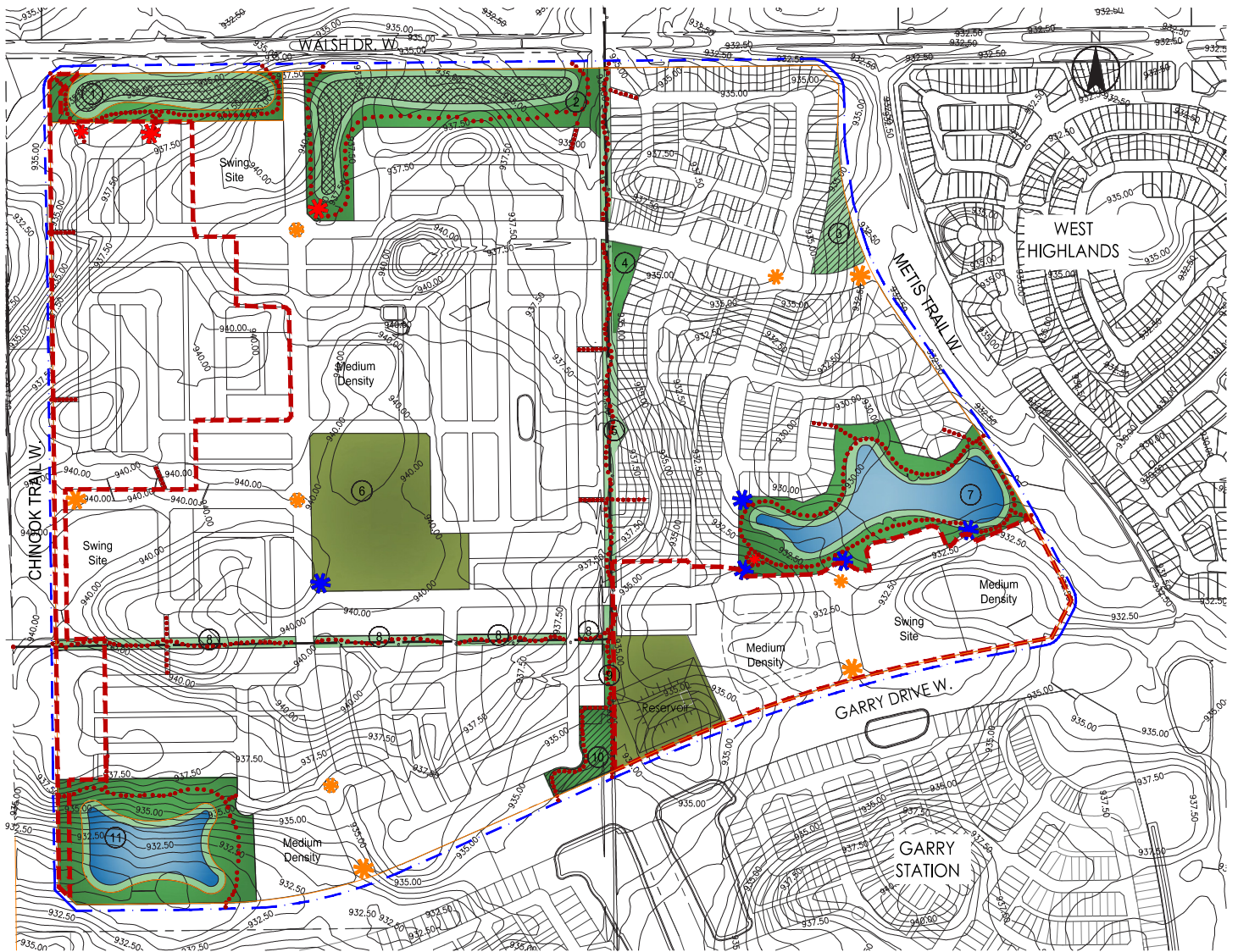
	Developable Area	Reserve Required	Reserve Provided	Percent of Developable Area
Country Meadows	±123.09 ha (±304.16 ac)	±12.31 ha (±30.42 ac)	±12.47 ha (±30.81 ac)	10.13%

Land Owner Breakdown	Reserve Provided	Percent of Developable Area
Southgate Commercial Lands Corp.	±2.53 ha (±6.25 ac)	2.06%
2014836 Alberta Ltd.	±2.64 ha (±6.52 ac)	2.14%
BW2 West	±3.94 ha (±9.74 ac)	3.20%
Debra L. Dudley-Olafson	±3.36 ha (±8.30 ac)	2.73%
City of Lethbridge	±0.23 ha (±0.57 ac)	0.19%
TOTAL	±12.47 ha (±30.81 ac)	10.13%

6.4 Seasonal Wet Areas

The developer shall submit Alberta Environment approval concurrent with any request to begin area grading on any seasonal wet area identified in supporting studies. Requirements can be found in the Provincial Wetland Restoration/Compensation Guide, Alberta Environment.

Figure 6.1: Open Space Network



Legend

- Country Meadows Amendment Boundary
- Public Building, Parks & Recreation (P-B)
- Parks and Recreation (P-R)
- Open Space (OS n/c)
- Stormwater Management Facility (Wet Pond)
- Stormwater Management Facility (Dry Pond)
- Stormwater Management Facility (Hybrid Pond)
- Local Connector Pathways
- Regional Multi-Use Pathway
- Themed Gateway Entrance to Park
- Themed Park Amenity Features
- Community Entry and Themed Elements
- PUL Public Utility Lot

<p>1. Modified Linear Park Creditable MR = 0.85ha</p>	<p>5. Linear Park Non-Creditable MR = 1.09Ha</p>	<p>9. Linear Park Creditable MR = 0.11Ha</p>
<p>2. Modified Linear Park Creditable MR = 1.71ha</p>	<p>6. School Site Creditable MR = 3.94Ha</p>	<p>10. Modified Pocket Park Creditable MR = 0.61Ha</p>
<p>3. Existing Dry Pond Non-Creditable MR</p>	<p>7. Existing Neighbourhood Park Creditable MR = 2.26Ha</p>	<p>11. Modified Linear Park Creditable MR = 2.56Ha</p>
<p>4. Linear Park Creditable MR = 0.27Ha</p>	<p>8. Linear Park Non-Creditable MR = 1.57Ha</p>	

CHAPTER 7: ARCHITECTURAL STANDARDS

The built form of the development will be subject to architectural standards and design guidelines. These guidelines will be initiated and implemented by the development team and will include design guidelines such as:

- Minimum/Maximum building footprints;
- Requirements for attached/detached garages;
- Fencing design and materials;
- Roofing materials;
- Diversity of building design;
- Exterior finish; and
- Landscaping requirements.

The detailed design guidelines will be developed and enforced at the subdivision stage of development. In general terms, the use of fieldstone and local materials will be promoted to establish a “Country Style” as outlined in **Section 5: Community Vision & Design**. It is anticipated that a “craftsman” or “country estate” style of architecture will be the focus of development east of 30th Street and that “ranch” style elements may be incorporated into neighbourhoods west of 30th Street.

CHAPTER 8: TRANSPORTATION

Figure 8.1: Preliminary Transportation Network has been updated to reflect the proposed amendment and identifies proposed roadway classifications, intersection spacings and Roundabout locations.

~~**Figure 9.2: Roundabouts: Lotting Concept & Restriction** has been deleted from future design criteria to reflect the current roundabout initiatives for Collector-Collector intersections within the City of Lethbridge. Refer to item 8.3 below: In areas where roundabouts are located on residential frontages, neither parking nor driveway access shall be permitted between the outside edge of crosswalk and the circulatory roadway. Roundabouts will be appropriately sized, accommodating WB-17 left turn movements, City Bus and Fire Truck U-Turn movements, with a minimum 4 metre diameter raised centre island; including mountable aprons.~~

A Transportation Impact Assessment (TIA) ~~reflects the proposed amendment~~ is included in **Appendix B**. The analysis indicates the additional units will have a minor impact to the anticipated operations of surrounding internal intersections and broader external intersections. The intersection geometries and traffic control measures previously assumed as part of the Country Meadows TIA are expected to be adequate to accommodate the proposed land use revisions.

8.1 Arterial Road Development, Drainage & Site Access

Country Meadows will be bound by **future** arterial roadways on all 4 of its boundaries. Metis Trail on the east and Garry Drive on the south boundary have undergone a functional design process which established the alignment and grade for Metis Trail and Garry Drive. Walsh Drive on the north boundary is existing and consideration of existing grades has been considered in the preparation of this document. The future Chinook Trail on the west boundary is yet to be designed; however, existing topography in the area has been considered in the development of grading and drainage.

Country Meadows has 5 access points as indicated in **Figure 8.1: Preliminary Transportation Network** and described below:

- Walsh Drive West: 1 access
- Metis Drive West: 1 access
- Garry Drive West: 2 accesses
- Chinook Trail West: 1 access

Emergency Services Access shall be provided to the development during all phases development. Please refer to **Section 10 Implementation** for information on Interim Secondary access location options.

Drainage from arterial roads has been accommodated up to the centre line of the adjacent arterial, drainage, and topography from beyond the centre line has been considered in the development of this Plan. Please refer to **Section 9.1 Stormwater Management** for more details.

8.2 Road Classifications

A circulation collector roadway has been established through previous planning at the Area Structure Plan level.

Centerline to Centerline spacing has been identified on the future collector roads shown on **Figure 8.1 Preliminary Transportation Network**. The following is a list of Roadway Classifications that have been proposed for Country Meadows. Refer to **Appendix B: Traffic Impact Assessment** for final roadway classifications.

- Community Entrances / Super Collector
- Major Collectors
- Minor Collectors
- Local Roads & Cul-de-sacs
- Lanes

~~8.3 Roundabouts~~

~~In areas where roundabouts are located on residential frontages, neither parking nor driveway access shall be permitted between the outside edge of crosswalk and the circulatory roadway. Roundabouts will be appropriately sized, accommodating WB-17 left turn movements, City Bus and Fire Truck U-Turn movements, with a minimum 4 metre diameter raised centre island, including mountable aprons.~~

8.3 Transportation Impact Assessment

A Transportation Impact Assessment (TIA) has been completed following City of Lethbridge TIA guidelines and roadway classifications have been determined using City of Lethbridge Design Standards 2011. Refer to **Appendix B: Traffic Impact Assessment and Community Entrance Noise Assessment**.

8.4 Noise Level Assessment

A Community Entrance Traffic Noise Assessment has been completed. The purpose of the assessment is to provide noise attenuation design requirements at the 10 year and, if required, 20 year horizon for the surface traffic resulting from the proposed Community Entrance Road connecting Metis Trail on the east side of Country Meadows.

Two scenarios were analyzed in the report: Scenario 1 - level lots layout assumptions and Scenario 2 - walkout lot substitutes. For both scenarios noise levels are expected below the City of Lethbridge LEQ noise limit of 60 dB(A) for roadways classified as non-truck routes in the 10 year horizon. Therefore, no sound attenuation measures are required for Country Meadows, and an analysis of the 20 year horizon data will not be required. Refer to **Appendix B: Traffic Impact Assessment and Community Entrance Noise Assessment** for further details of the assessment.

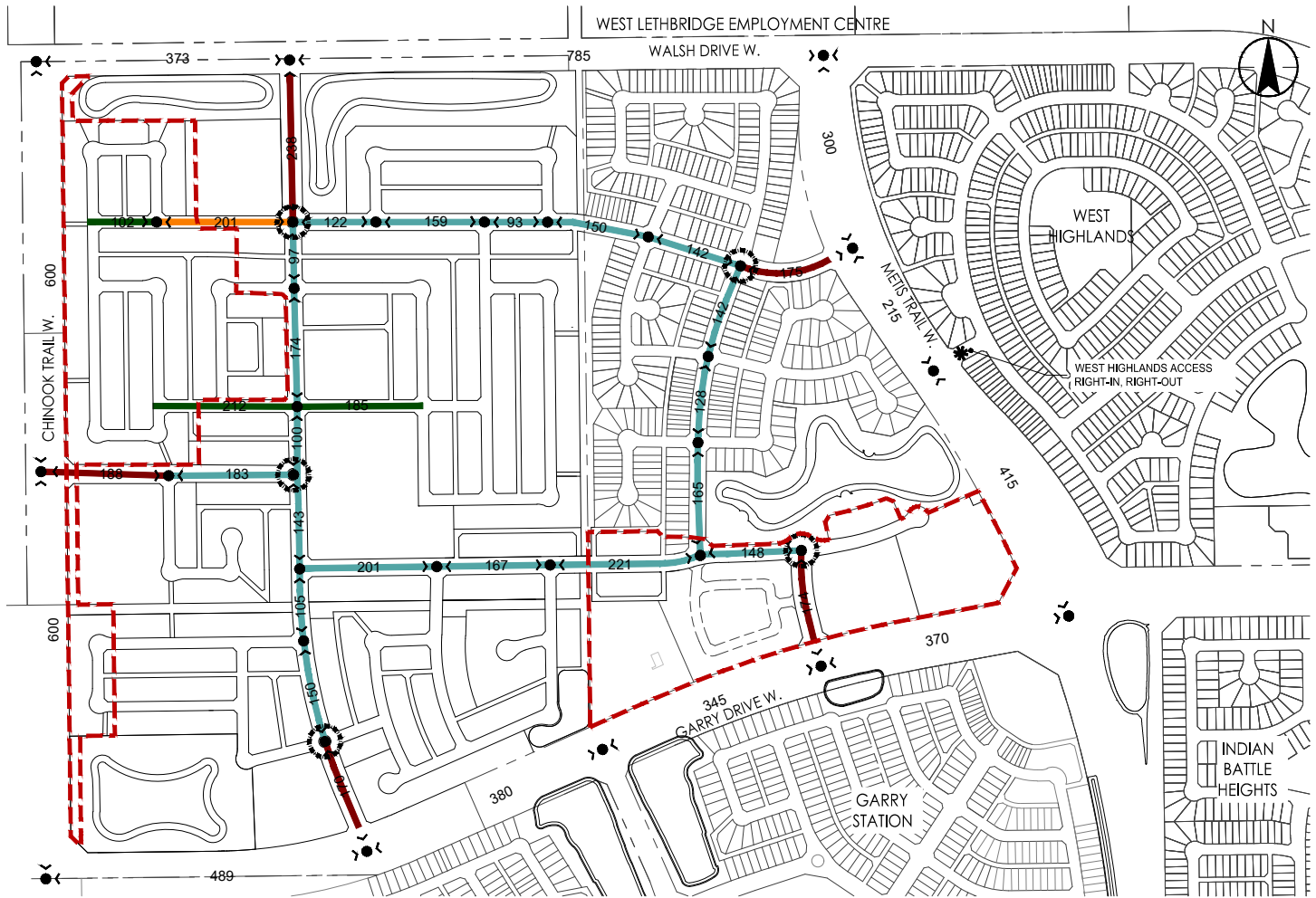
8.5 Chinook Trail & Community Entrance

~~The completion of the Chinook Trail Right of Way has been reduced to 60.0m, however, this entrance into Country Meadows from the transportation perspective is not anticipated to be completed in the near term. However, Infrastructure installations, however, may occur along Chinook Trail at any time in order to advance the development within County Meadows as required. (high pressure gas line relocation and overhead electrical transmission lines) are anticipated in the near term within this future Right of Way.~~

8.6 Transit Routes

Transit Routes shall be planned for bi-directional service utilizing collector roads. Transit routing is at the discretion of the City of Lethbridge Transit Department and subject to the City's transit master plan.

Figure 8.1: Preliminary Transportation Network



Notes: Refer to Appendix B, TIA Detailed Roadway Analysis

- Legend
- Country Meadows Amendment Boundary
 - Community Entrance/Super Collector
 - Major Collector
 - Minor Collector
 - 18.5m Local Road (11m Asphalt)
 - Roundabout
 - <100m >● Intersection Spacing

~~CHAPTER 9: TRANSIT & MAILBOX SERVICES~~

~~9.1 Transit Routes~~

~~Transit Routes shall be planned for bi-directional service utilizing collector roads. Transit routing is at the discretion of the City of Lethbridge Transit Department and subject to the City's transit master plan.~~

~~9.2 Community Mailbox Locations~~

~~The final location of community mailboxes will be determined in conjunction with Canada Post at the time of detailed design. Community mailboxes are typically located adjacent the City sidewalk along the long frontage of corner lots, or along the openings to park space. Transit bus stop locations will not be combined with community mailbox sites.~~

CHAPTER 9: SITE SERVICING

The following sections provide a brief overview of the servicing strategy for Country Meadows. It is understood that final designs at the time of subdivision will adhere to current municipal and provincial standards.

9.1 Stormwater Management

Background Information

Where practical, catchment areas have been defined by natural topography in an effort to minimize excessive earthwork; these boundaries extend to the centerline of ~~the adjacent arterial roadways~~ **Walsh Drive to the north** thereby allowing for the combined control of runoff from the development and arterials with the exception of the northeast corner of Country Meadows, where existing topography and functional road design make a combined Stormwater Management Facility prohibitive. **The natural drainage course for Lethbridge County lands west of Chinook Trail is to the west coulees. At the time of report preparation, a portion of the undeveloped lands within Country Meadows also flows to the west. As directed by the City, the Plan Area will be regraded to direct flows to the east, containing all flows within the municipal boundary. Chinook Trail will become the high point dividing the flows east and west. At time of development, the Walsh Drive Functional Plan and Chinook Trail Functional plan should be reviewed for future road grade tie ins & stormwater management catchment areas.**

Due to offsite constraints, Country Meadows storm water ponds will need to operate at a “zero” release rate. The City has indicated that Country Meadows storm water detention facilities will need to connect to the 1800mm diameter storm line that extends from the west boundary of West Highlands along the projection of Tartan Boulevard. Record drawings indicate that the depth of this existing line should be

sufficient in order to service the western boundary of the development.

The City has indicated that storage of 1000m³/ha should be allowed for at the Outline Plan stage of planning. This is a volume equivalent to the 90% of the 1:100 year, 24 hour rainfall volume (~110mm rainfall). This is based on no allowance for initial abstraction, depression storage or infiltration.

Design Assumptions

The development area has been divided into 6 catchments that drain into 6 ponds. The ponds and catchment areas have been identified on **Figure 9.1: Stormwater Management Major System.**

As specified in the City of Lethbridge Design Standards, storm water ponds must be designed to fully accommodate runoff from the 1:100 year, 24 hour rain event. As shown in **Table 2**, the ponds are capable of accommodating this volume.

With regard to pond discharge, it is anticipated that all pond outlets will connect to a future sewer trunk that will extend from the existing 1800mm diameter pipe located at the west end of Tartan Boulevard in the West Highlands Area. All ponds will be serviced by a minor storm sewer system sized to convey runoff from up to the 1:5 year rainfall event.

Design Criteria used for the major and minor storm sewer systems are:

- 1 000m³/ha of detention storage
- 200 L/s/ha for Major System Flows

- 90 L/s/ha for Minor System Flows

Overland flow routes are to be designed to convey the 1:100 year storm event and not exceed Alberta Environment guidelines for safe velocities and depths. Overland flow routes will incorporate trapped lows at strategic locations. Trapped low areas will:

- Increase surface run-off capture
- Provide for energy dissipation during extreme rain fall events (“stilling” basins)
- Allow for the practical creation of overland flow routes given localized topographical constraints.
- Meet City of Lethbridge design guidelines for maximum depth of 300mm.

In addition to the above, overland flow within a drainage boundary has been proportioned in a way that evenly distributes overland flow routes throughout the drainage boundary. This has been completed to reduce the cumulative effects of long overland flow routes. Special attention at the detailed design stage may be required where two intersecting overland flow routes meet. Where possible, this point of intersection should occur in close proximity to a storm water management facility.

The final design of overland flow routes must be confirmed during the detailed design of a storm water management facility and its upstream catchment.

In areas where the back of lots drain to an arterial right of way, the City will require (at subdivision and detailed design) that either the arterial road and associated drainage system be constructed to convey storm water to an approved detention facility, or, in the absence of arterial road development, the lots be designed to drain from back to front.

Proposed Storm Water Management Infrastructure

~~Stormwater will be managed within the Plan Area to accommodate all open space and residential development within Country Meadows. All pond catchments follow natural topography where possible. This amendment proposes minor changes to storm water management to accommodate the reconfiguration of open space and the additional residential development. The pond catchments have~~

~~been revised to follow natural topography where possible.~~

Future Pond Designs must consider water quality best practices including the supply of make-up water and effects of solar heating. Refer to **Figure 9.2: Stormwater Management Minor System** for proposed make up water supply line alignment. Final determination of pond areas, volumes and upstream catchments will be required during detailed master servicing design and subdivision. It is recommended that other stormwater management strategies and systems be investigated at detailed design including combination facilities, treatment forebays, wetlands and green infrastructure (low impact design techniques). These facilities have been identified as hybrid ponds. Provision for a make-up water system from Garry Drive should be implemented along the central north-south green strip.

Pond A is an existing wet pond that discharges to an existing storm trunk extending from Tartan Boulevard. Pond catchment areas will remain unchanged.

Pond B is an existing dry pond that discharges to an existing storm trunk in Metis Trail. Pond catchment areas will remain unchanged.

Pond C has been designated a Hybrid Pond. The facility’s location has been chosen due to natural topography and its service boundary has increased and has been accounted for in Table 1. Ultimately, the facility will discharge to a future trunk line in Walsh Drive as identified in the Walsh Drive Preliminary Design Report November 2012 (drawings included in **Appendix M: Walsh Drive Preliminary Design Report** drawings). In the interim, this pond will discharge to Country Meadows Boulevard via a temporary lift station.

Pond D has been designated a Hybrid Pond. The facility's location has been chosen due to natural topography and its service boundary has increased and has been accounted for in Table 1. Ultimately, the facility will discharge to a future trunk line in Walsh Drive as identified in the Walsh Drive Preliminary Design Report November 2012. In the interim, this pond will discharge to Country Meadows Boulevard via a temporary lift station and be connected to Pond C via a large diameter pipe.

Pond E has been designated a Wet Pond. The facility's location has been chosen due to natural topography and its service boundary has decreased and has been accounted for in Table 1. The facility will discharge to a future trunk line in Metis Trail.

Pond F has been designated a Dry Pond and has been created due to natural topography and the functional design grades of Garry Drive. The facility will discharge into a future storm trunk line in Garry Drive.

Figure 9.1: Storm Water Management Major System has been updated to reflect the proposed amendment and future pipe information has been moved and added to an additional Figure 9.2 Storm Water Management Minor System to add clarity.

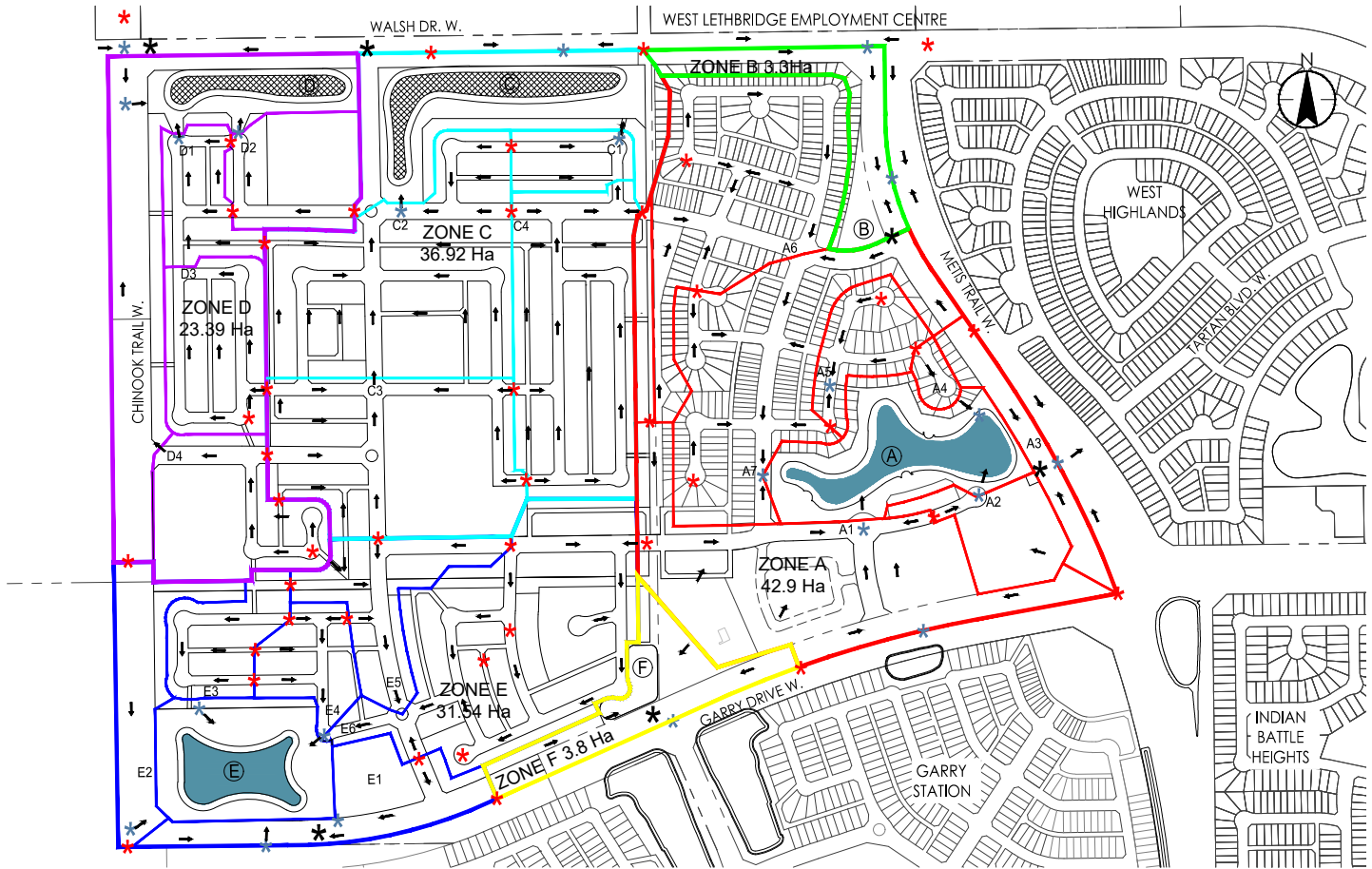
Table 2: Pond Statistics

Drainage Boundary & Pond	Pond Type	Catchment Area	Active Storage Pond Volume
A	Wet	42.9 ha	42,900 m ³
B	Dry	3.3 ha	3,300 m ³
C	Hybrid	36.9 ha	36,900 m ³
D	Hybrid	21.1 ha	21,100 m ³
E	Wet	31.1ha	31,100 m ³
F	Dry	3.8 ha	3,800 m ³

Table 3: Minor & Major Storm Flows

Catchment		Sub Catchment*		Minor System Flow m3/s	Major System Flow m3/s
ID	Area (ha)	ID	Area (ha)		
A	42.9	A1 > Pond	10.7	1.0	2.1
		A2 > Pond	2.6	0.2	0.5
		A3 > Pond	2.0	0.2	0.4
		A4 > Pond	0.5	0.05	0.1
		A5 > Pond	2.3	0.2	0.5
		A6 >	9.2	0.8	1.8
		A7 > Pond	9.0	0.8	1.8
		Subtotal	18.2	1.6	3.6
		Pond "A" Area	6.6	N/A	N/A
Total Area "A"	42.9	N/A	N/A		
B	3.3	Total Area "B"	3.3	0.3	0.7
C	36.9	C4 >	8.6	0.8	1.7
		C1 > Pond	1.7	0.1	0.3
		Subtotal	10.3	0.9	2.0
		C3 >	8.7	0.8	1.7
		C2 >Pond	10.7	1.0	2.1
		Subtotal	19.4	1.8	3.8
Pond "C" Area	7.2	N/A	N/A		
Total Area "C"	36.9	N/A	N/A		
D	21.1	D3 >	4.0	0.4	0.8
		D1 >Pond	2.3	0.2	0.5
		Subtotal	6.3	0.6	1.3
		D2 > Pond	3.4	0.3	0.7
		D4 > Pond	4.8	0.4	1.0
		Pond "D" Area	6.6	N/A	N/A
Total Area "D"	21.1	N/A	N/A		
E	31.1	E1 > Pond	3.8	0.3	0.7
		E2 > Pond	2.9	0.3	0.6
		E3 > Pond	2.5	0.2	0.5
		E4 > Pond	2.1	0.2	0.4
		E5 >	3.5	0.3	0.7
		E6 > Pond	11.5	1.0	2.3
		Subtotal	26.3	2.3	5.2
		Pond "E" Area	4.8	N/A	N/A
Total Area "E"	31.1	N/A	N/A		
F	3.8™	Total Area "F"	3.8	0.3	0.8

Figure 9.1: Stormwater Management Major System



Notes: Trapped lows have not been indicated but will be incorporated at strategic locations during detailed design.









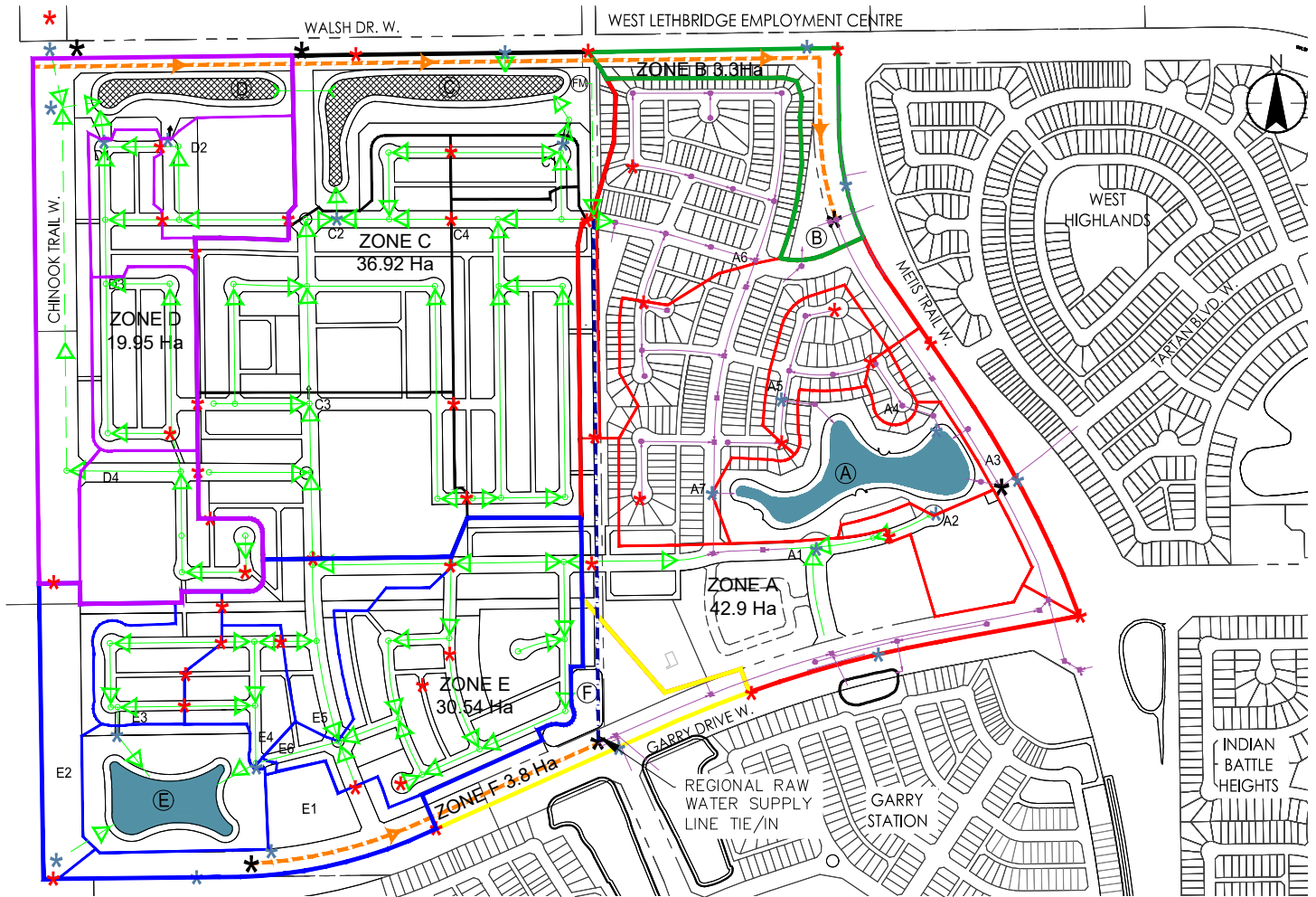
- | | |
|-------------------------------------------------------------------------------------|-------------------------|
| Legend | |
|  | Overland Flow Direction |
|  | High Point |
|  | Low Point |
|  | Pond Identification |
|  | Sewer Connection Points |
|  | Wet Pond |
|  | Dry Pond |
|  | Hybrid Pond |

Figure 9.2: Stormwater Management Minor System



Notes: Trapped lows have not been indicated but will be incorporated at strategic locations during detailed design.

- | | |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <p>Legend</p> <ul style="list-style-type: none"> → Overland Flow Direction * High Point * Low Point (E) Pond Identification * Sewer Connection Points | <ul style="list-style-type: none"> —▶ Future Storm Trunk —▶ Future Storm Sewer and Flow Direction —▶ Existing Storm Sewer and Flow Direction (FM) Storm Water Lift Station --- Storm Forcemain --- Pond Make-up Water Supply Line |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

9.2 Sanitary Servicing

Background Information

Previous planning documents have indicated that substantial upgrades or new installations will be required to the sanitary sewer collection system on the City of Lethbridge's West Side in order to provide adequate service to future developments. It is understood that the City will construct a new sanitary sewer trunk line through the Bridge Drive Utility Corridor that will be installed along Walsh Drive, Metis Trail and Garry Drive. The proposed sewer trunk in Metis Trail will match the existing 600mm diameter sewer that extends west of West Highlands along the projection of Tartan Boulevard.

Initially, the Outline Plan area will be provided sanitary sewer service by connecting to the existing sanitary sewer trunk extending into Metis Trail from Tartan Boulevard. This connection will allow the first phase of development to proceed. Upon completion of the Bridge Drive Utility Corridor a total of 60 ha of development shared between the Country Meadows and Garry Station plan areas can be served through the Tartan Boulevard trunk. Ultimately, capacity will be provided through a new sanitary sewer trunk running east along Walsh Drive and north along Metis Trail. The ultimate servicing trunks will be constructed as required to meet development demand.

The Country Meadows ASP indicates that a 1200mm diameter sewer trunk will be extended from Walsh Drive along Metis Trail and that a 450mm diameter sewer trunk will be installed along Garry Drive as part of the City's long range Capital Improvement Plan.

Design Standards

City of Lethbridge Design Standards for residential flows has been used for analysis.

- Dry Weather Flow: 500L/cap/day
- Wet Weather Flow: 400L/cap/day
- Infiltration: 150L/cap/day
- Harmon's Peaking Factor: $[14 / (4 + \sqrt{P})] + 1$

Sanitary Servicing

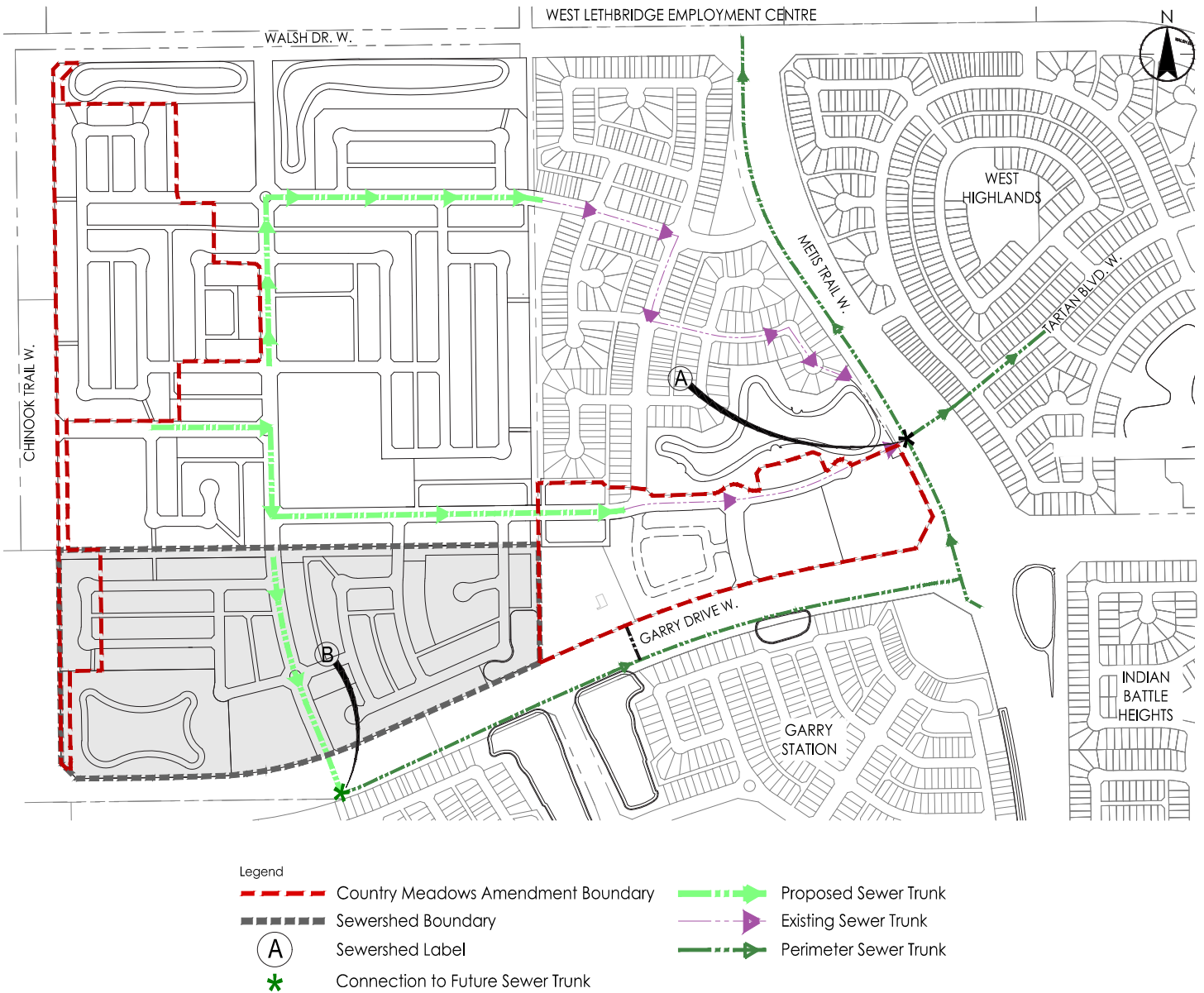
The sanitary servicing strategy remains largely unchanged aside from the additional residential development. The revised population statistics for Country Meadows indicate a total residential population of **6017** people. The revised sanitary sewage flow estimates have been identified by sewershed in **Table 4** below. A total peak sewage flow of **122** L/s was identified in **2019**, and this total flow has increased to **123.1** L/s based on new population. Based on the as-built sewer trunk exiting Country Meadows at Metis Trail (Ø400 @ 0.4%), the system has a capacity of 132L/s which is sufficient for the servicing of Sewershed A. Sewershed B will be serviced in the future by a sewer extension along Garry Drive west of 30th Street.

Figure 9.3: Sanitary Servicing & Connection Points has been updated to reflect the proposed amendment.

Table 4: Sanitary Sewage Flow Estimates by Sewershed

Sewershed	Gross Area (ha)	Population	Peak Flow (L/s)
A	89.94	4987	103.8
B	24.15	1030	24.0
Total	123.09	6017	123.1

Figure 9.3: Sanitary Servicing & Connection Points



9.3 Water Servicing

Background Information

The servicing of Country Meadows, from the perspective of potable water, ~~is will be ultimately~~ supported by the ~~existing development of a~~ Treated Water Reservoir/Pumping Station that ~~is will be~~ located on the north side of Garry Drive within the Country Meadows Outline Plan Boundary.

~~Construction of this reservoir is currently underway.~~

The development ~~is will be~~ flanked by major ~~transmission distribution~~ lines--specifically a ~~future~~ 600mm diameter water line in Garry Drive and ~~the existing~~ 400mm diameter water line located ~~along in the future~~ Metis Trail. In the future, water ~~transmission distribution~~ lines will extend along Walsh Drive and Chinook Trail as well.

Table 5: Estimated Water Demands

Average Day Demand (415L/cap/day)	2.42 ML/day
Maximum Day Demand (2.2 x ADD*)	5.32 ML/day
Peak Hour (3.5 x ADD)	8.47 ML/day

*ADD – Average Day Demand

Note: Water Usage based on an estimated population from Land Use Statistics.

Design Standards

The following acceptable delivery pressures are stated in the **2021** City of Lethbridge Design Standards, Level of Service Objectives:

- No less than 310 kPa (45 psi) during peak hour demand
- No less than 345 kPa (50 psi) at maximum day demand
- Maximum delivery pressure will not exceed 620 kPa (90 psi)

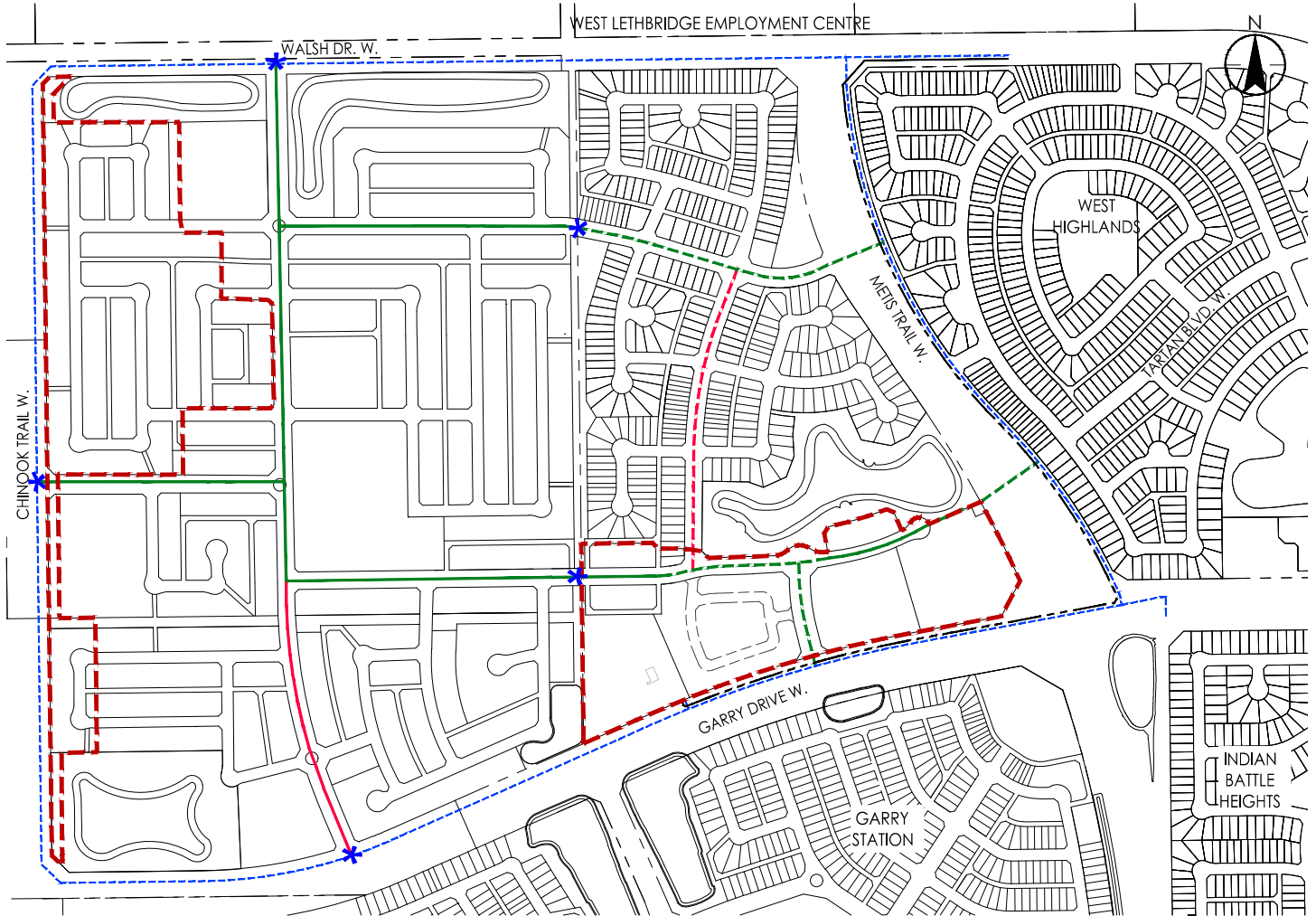
The water system must also be able to supply adequate flow to hydrants. A minimum of 75 L/s with a minimum residual pressure of 150 kPa must be maintained during maximum day demand. Specific land uses and structures may require higher fire flows. The distribution system must be able to meet the requirements described in “Water Supply for Fire Protection” published by Fire Underwriters Survey.

Water Servicing

The water servicing strategy remains largely unchanged aside from the additional water demand on the system due to the additional residential development. **Table 5** identifies the revised estimated water demands.

Figure 9.4: Water Servicing & Connection Points has been updated to reflect the proposed amendment.

Figure 9.4: Water Servicing & Connection Points



- Legend
- - - Country Meadows Amendment Boundary
 - Proposed 250Ø
 - Proposed 300Ø
 - - - Perimeter Water Distribution
 - - - Reservoir Fill Line
 - * Connection to Future Water Trunk
 - - - Existing 250Ø
 - - - Existing 300Ø

9.4 Shallow Utilities

Existing Infrastructure

ATCO PIPELINES

ATCO Pipelines has been contacted with regard to the integration and/or relocation of their existing high pressure and distribution lines in the Country Meadows Area. ~~Discussions between the Developer, ATCO Pipelines and the City of Lethbridge will be continued during the development of Country Meadows. At this time, it is proposed that Public Utility Corridors be established along the existing pipe alignments combined with pathways. We wish to note that this proposal does not prevent future developer's from relocating the line through a planning adjustment.~~ During discussions between the Developer and ATCO Pipelines on July 23, 2024, ATCO noted that existing lines within the Plan Area are anticipated to be adjusted beginning in 2025. Details of the segmented lines are noted within **Figure 9.5: ATCO Transmission & Distribution Plan.**

The existing Regulating Station on the east side of Segment 1 along Metis Trail is anticipated to be relocated north of Walsh Drive in 2025. Once this is completed, Segment 1 is anticipated to be abandoned in place. At time of construction, the abandoned pipe may be removed and disposed of by the developer.

The High Pressure (transmission only) line within Segments 2 & 3 will remain in service for the foreseeable future. At the time of report preparation, ATCO does not have a plan to relocate these segments. When Chinook Trail is constructed, the City of Lethbridge has indicated an interest in relocating the transmission line within the ROW, if possible. In the interim, a new distribution line will be connected from the NE corner of the northern parcel to ensure continued service.

Segment 2 Distribution line, located on the west side of the 30th Street ROW, will be abandoned in place. In order to service the Dudley-Olafson parcel, a new service line will be connected from the SE corner.

In 2024, ATCO will be assessing their greater network to determine if the blow down valve, currently located along the western boundary, is required. At the time of report preparation, the City and developer both request the valve to be relocated. If it is determined that it must remain, consideration should be given to relocating it outside of the Plan Area, in order to remove all ATCO infrastructure from developable lands within Country Meadows.

ATCO has provided Stantec with their "ATCO Pipelines Guidelines Controlling Development and/or Landscaping of High Pressure Natural Gas Rights of Way" (enclosed). It is anticipated that the Developer/City of Lethbridge will submit plans to ATCO Pipelines during the Gate 3 design stage. **Until removal is completed**, ATCO's current easement widths must be maintained along with cover above the existing line. However, the addition of fill material above the line will be permitted to a height of 2m above the crown of the gas line. Minimum cover is 1.2m.

Correspondence with ATCO Pipelines indicates that an easement currently registered to Canadian Western Natural Gas on SW 34-8-22-4 has no corresponding infrastructure installed within it and has been discharged.

It is understood that City of Lethbridge Land Use Bylaws will govern the development of land beyond the gas line right of way (buildings shall be a minimum of 15.25m from the gas line).

ATCO GAS

It is anticipated that the existing ATCO gas facilities will be relocated and integrated into the community at the subdivision detailed design stage.

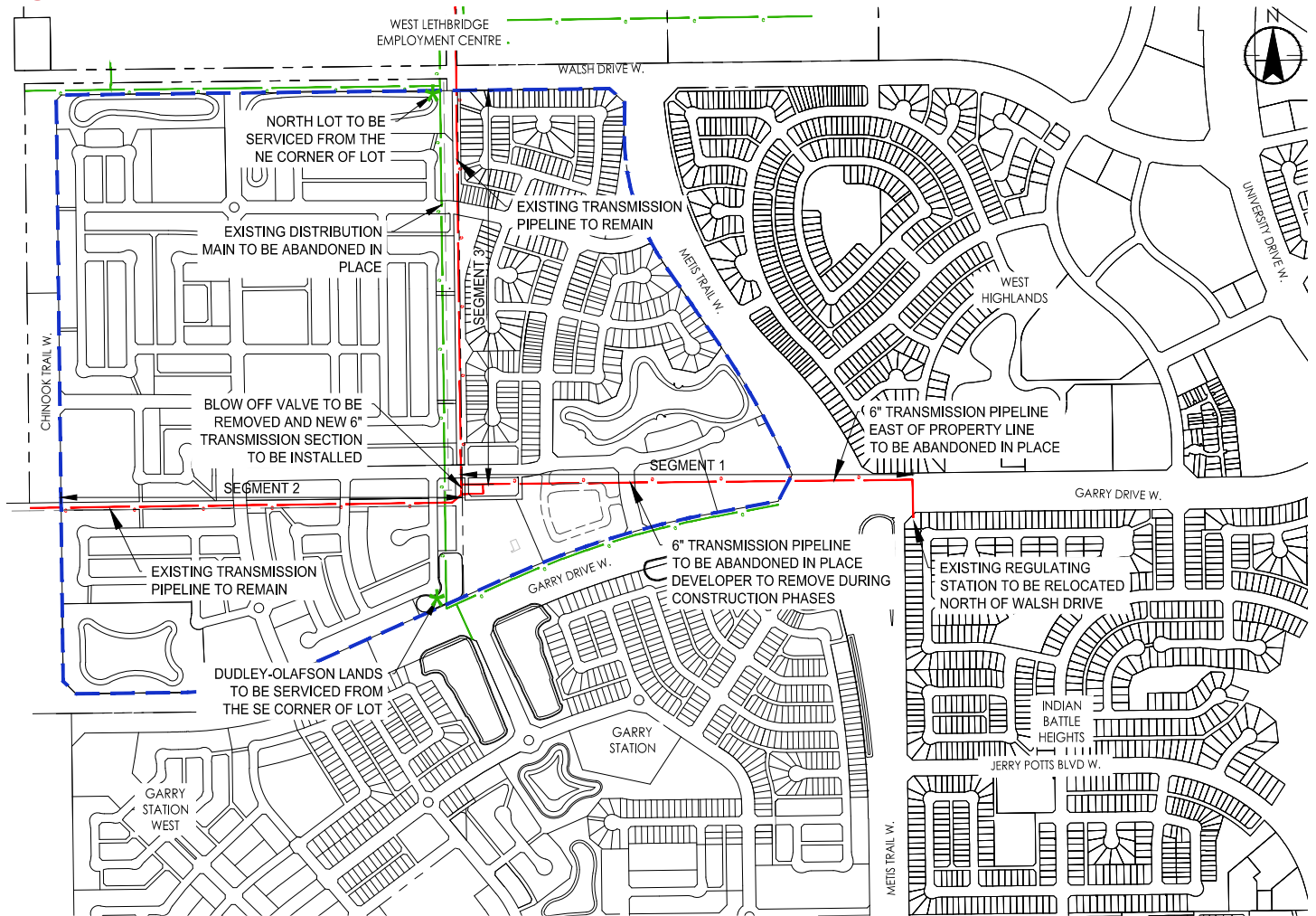
TELUS

It is anticipated that the existing Telus facilities will be relocated and integrated into the community at the subdivision detailed design stage.

FORTIS

It is anticipated that the existing Fortis facilities will be relocated and integrated into the community at the subdivision detailed design stage by the City of Lethbridge and their Electrical Department.

Figure 9.5: ATCO Transmission & Distribution Plan



Legend

- — — Country Meadows Boundary
- — — Country Meadows Amendment Boundary
- • — ATCO Gas (Distribution)
- • — ATCO Pipelines (High Pressure)
- ✱ New Service Connection

CHAPTER 10: PROPOSED STAGING IMPLEMENTATION

~~Figure 12.1: Proposed Phasing~~ has been renamed to **Figure 10.1: Proposed Development Staging**. This change will give more flexibility to the developer at time of subdivision to adjust their phase size as required, based on current market conditions. Staging will utilize the existing infrastructure, and the construction of critical new infrastructure including storm water management facilities, to provide logical extensions to the community.

Table 6: Staging Requirements

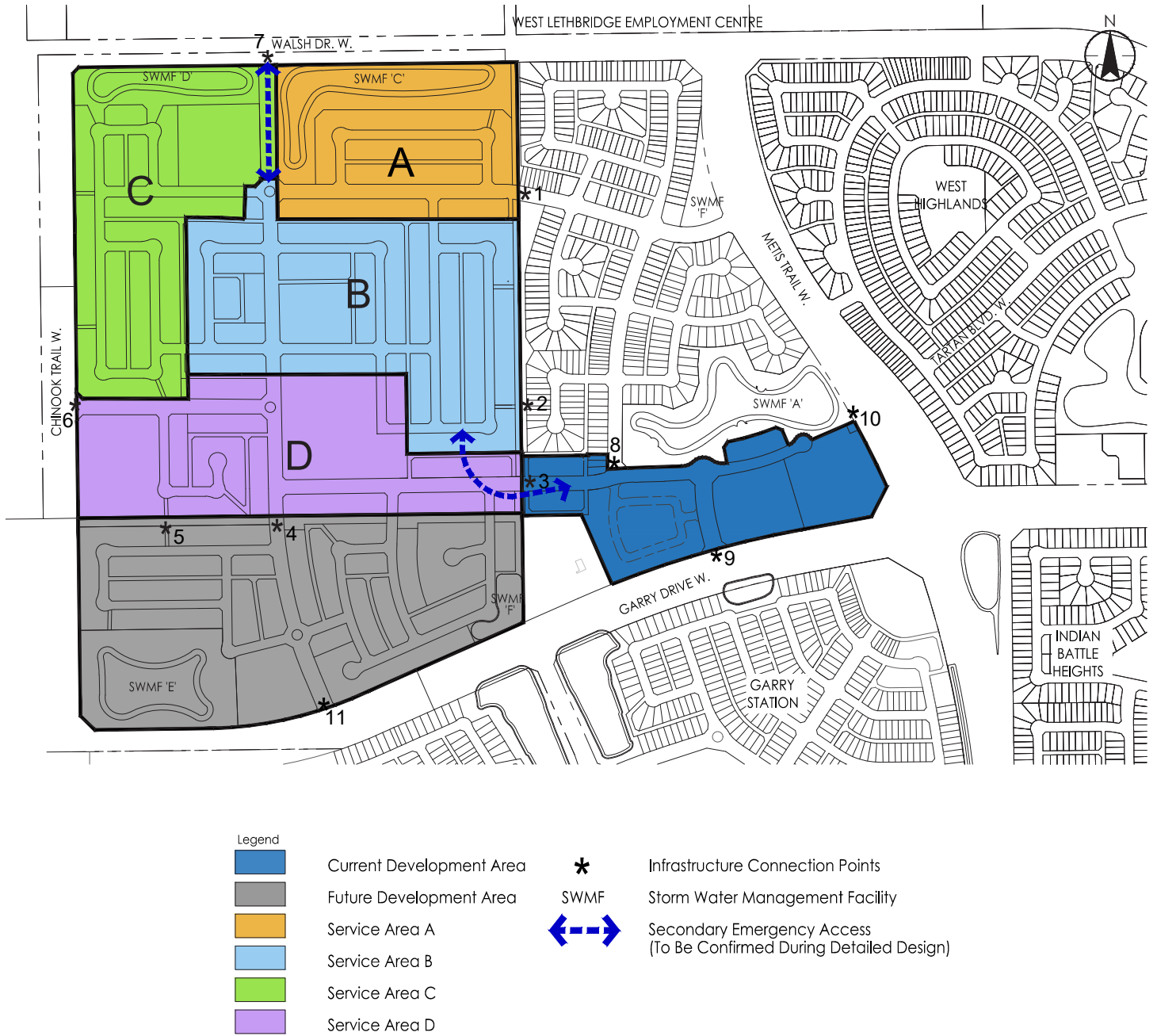
Area	SWMF	Water	Sanitary	Transportation
Current	A	8,10	10	8,9
A	C	1	1	1
B	A	1,2	1	1
C	D	1,2,6	1	1,7
D	C,D,[E,F]	1,2,3	1	1,3,7
Future	E,F	4,11	4,11	4,11

At time of development, infrastructure looping and road accesses will be reviewed based on the most current City of Lethbridge Standards. Phased construction shall meet with current City guidelines with respect to Water Distribution, Sanitary and Storm Sewer Systems, and Storm Water Management Facilities. Offsite Infrastructure may be required to facilitate development, Ponds and pond outlets must be constructed concurrently with the first phase requiring the facility. All phases will provide overland flow routes to the ponds.

School site development within Country Meadows would occur during the development of Stage D based on the logical extension of utility servicing from the north. Accelerated development of the school site would be at the discretion of the land owner and subject to the ability to bring electrical and storm water management infrastructure to the site.

Regarding secondary emergency services access, Areas A & B will require a secondary access at some point during subdivision phasing. Two options have been identified for a temporary secondary emergency access. The final location and design of this interim access will occur at subdivision and detailed design in consultation with City of Lethbridge Emergency Services.

Figure 10.1: Proposed Development Staging



CHAPTER 11: FIRE PROTECTION

Public roadways will be designed to meet the current City of Lethbridge Design Standards to ensure safe emergency vehicle passage.

The City of Lethbridge “Emergency Response Time Modeling” for Country Meadows will need to be reviewed at the time of subdivision in order to meet applicable provincial regulations. Refer to **Appendix H: High Intensity Fire Response Analysis City of Lethbridge**.

CHAPTER 12: SUSTAINABILITY

Country Meadows has incorporated principles of sustainable design into the community, **aligning with the policies within the MDP. These policies support the vision of creating a well designed city that enhances and sustain a quality built environment to ensure future economic, environmental and social sustainability.** A variety of land use districts have been selected throughout the plan area to accommodate a mix of housing styles to address a wide range of market demographics. The land uses also provide opportunities to implement innovative design and creativity.

A comprehensive open space network achieves connectivity while creating a sense of place which promotes active and passive recreational opportunities as well as promoting alternative modes of transportation. The pedestrian and pathway network constructed of sustainable materials connects future residents with the amenities to the south. Bicycling friendly pathways and amenities will further enhance opportunities for neighbourhood socializing and interaction.

In the interest of sustainable development, high maintenance park areas have been minimized and xeriscaping principles will be used to create an aesthetic and functional open space network. Consideration of different lighting forms and power sources will be initiated as well as provisions for recycling sites. Building designs will also be explored to provide orientation for maximum feasible use of solar design and equipment.

The layout of the community was strategically designed to minimize grading, including locating storm water management facilities in natural low areas. Storm Water Management Facilities will be utilized to irrigate open space areas along with make-up water from the Lethbridge Northern Irrigation District: Refer to **Appendix G: Lethbridge Northern Irrigation District Water Conveyance Letter.**

CHAPTER 13: CONCLUSION

The Country Meadows Outline Plan Amendment continues to provide a logical extension to development in the communities of Garry Station, The Crossings and The Piers. The design of the Outline Plan conforms to the policies and intent of the Country Meadows Area Structure Plan. The Plan incorporates a variety of residential land uses providing the flexibility to incorporate innovative housing design and concepts. The extensive open space has created a community that offers modal choices to future residents.

Inspired by the early settlement of Western Canada, a new landscape of “country living” emerges to create a community representative of a pioneering spirit driven by family values. Here east meets west - where manicured two storey homes surrounded by white picket fences transition subtly with the introduction of the timber and fieldstone ranching lifestyle.

Southgate Commercial Lands Corp. respectfully requests OLP Amendment approval by the Assets and Infrastructure Standing Policy Committee (AISPC) and the Plan Review Team (PRT) to accommodate the continued development of Country Meadows. Following AISPC Acceptance, submission to City Council with recommendations to Approve, Land Use Bylaw Amendments and re-designations will be completed in the future at the discretion of the developer.